CONSTRUCTION PLANS COUNTY OF GRAND, TOWN OF FRASER, STATE OF COLORADO

KINGS CROSSING ROAD TOWNSHIP 1 SOUTH, RANGE 75 WEST OF THE 6TH PM, SECTION 28,

AGENCY CONTACTS:

OWNER:

GRAND PARK DEVELOPMENT LLC P.O. BOX 30 WINTER PARK, COLORADO 80482 PHONE: (303) 972-6633 CONTACT: CLARK LIPSCOMB

CIVIL ENGINEER

PHELPS ENGINEERING SERVICES 7200 E. HAMPDEN AVE. SUITE 300 DENVER, CO 80224 PHONE: (303) 298-1644 CONTACT: LONNY PHELPS

TOWN

TOWN OF FRASER P.O. BOX 370 **153 FRASER AVENUE** FRASER, COLORADO 80442 PHONE: (970) 726-5491 CONTACT: JEFF DURBIN

TOWN OF FRASER ENGINEER

C/O JRS ENGINEERING CONSULTANT, LLC 6013 BRIARWOOD DRIVE CENTENNIAL, COLORADO 80112 PHONE: (303) 726-5577 CONTACT: JIM SWANSON

SURVEYOR

DAVID EVANS AND ASSOCIATES 1331 17th STREET, SUITE 900 DENVER, COLORADO 80202 PHONE: (970) 531-8499

WATER AND WASTEWATER

PUBLIC WORKS P.O. BOX 125 FRASER AVENUE FRASER, COLORADO 80442 PHONE: (970) 726-5491 CONTACT: ALAN NORDIN

FIRE PROTECTION

EAST GRAND FIRE PROTECTION DISTRICT #4 P.O. BOX 2967, 77601 US HWY. 40 WINTER PARK, COLORADO 80482 PHONE: (970) 726-5824 CONTACT: TODD HOLZWORTH - CHIEF egfd@eastgrandfire.com

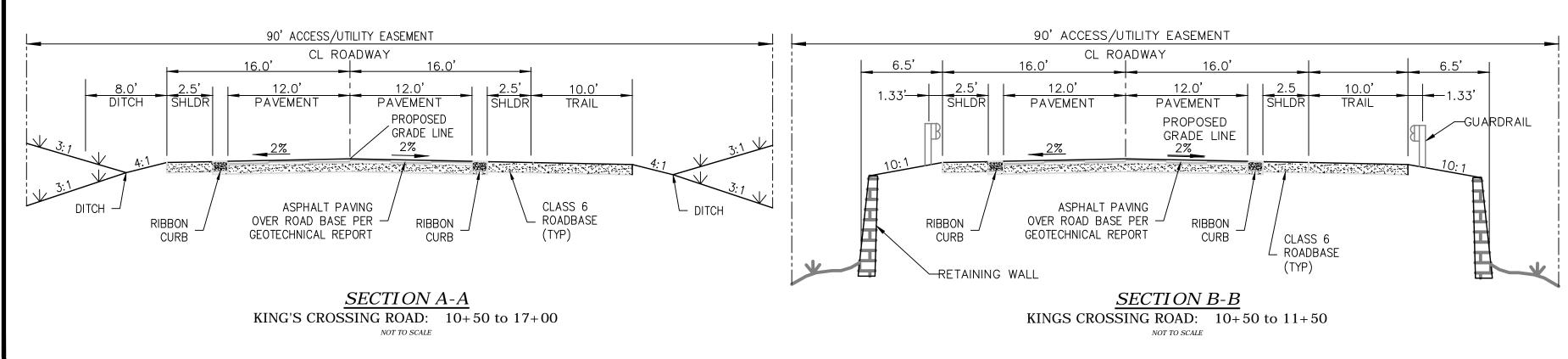
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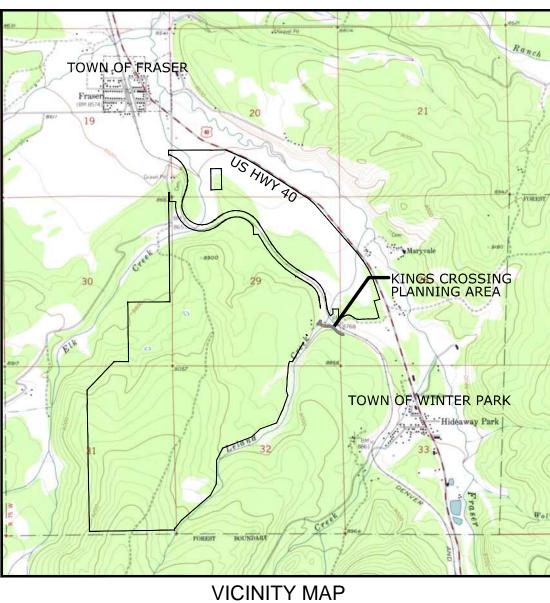
XCEL ENERGY P.O. BOX 528 GRANBY, COLORADO 80446 PHONE: (970) 887-3846 CONTACT: GARY SALBERG

ELECTRIC MOUNTAIN PARKS ELECTRIC, INC. P.O. BOX 170 GRANBY, COLORADO 80446 PHONE: (970) 887-3378 CONTACT: TODD CLAUSEN

CABLE TV

COMCAST P.O. BOX 785 GRANBY, COLORADO 80446 PHONE: (970) 887-2175 CONTACT: CHRIS MARCH





SCALE: 1" = 3000'

SHEET NO.	SHE
1	COVER SHEET
2	GENERAL NOTES &
3	EROSION CONTROL
4	EROSION CONTROL
5	KINGS CROSSING R

UTILITY NOTES _ PLAN NOTES AND DETAILS KINGS CROSSING ROAD PLAN AND PROFILE KINGS CROSSING ROAD PLAN AND PROFILE

TOWN OF FRASER APPROVAL BLOCK

THESE DESIGNS, PLANS, AND CONTRACT DOCUMENTS ARE REVIEWED FOR CONCEPT AND GENERAL CONFORMANCE TO THE TOWN'S MINIMUM STANDARDS ONLY, AND THE RESPONSIBILITY FOR DESIGN ADEQUACY SHALL REMAIN WITH THE ENGINEER OF RECORD. THIS REVIEW DOES NOT IMPLY RESPONSIBILITY BY EITHER THE TOWN OF FRASER OR THE TOWN'S ENGINEER FOR COMPLETENESS, ACCURACY OR CORRECTNESS OF CALCULATIONS. THE REVIEW DOES NOT IMPLY THE QUANTITIES OF ITEMS INDICATED ON THE PLANS ARE THE FINAL QUANTITIES REQUIRED. THE REVIEW SHALL NOT ACCEPTANCE OF FINANCIAL RESPONSIBILITY BY THE TC **REVIEWING PARTIES FOR ADDITIONAL ITEMS AND ADDIT** THAT MAY BE REQUIRED DURING THE CONSTRUCTION P

APPROVED	FOR CONSTRUCTION WITHIN ONE (1) YEAR O
BY	
	TOWN ENGINEER
BY	
	TOWN WATER / WASTEWATER SYSTEM ENGINEER

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	TOWN OF FRASER	
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EAST GRAND FIRE PROTECTION DISTRICT #4

"I HEREBY ATTEST THAT THESE CONSTRUCTION PLANS FOR KINGS CROSSING ROAD -HAS BEEN PREPARED BY ME OR UNDER MY DIRECT SUPERVISION, AND TO THE BEST OF MY KNOWLEDGE AND ABILITY HAS BEEN PREPARED IN ACCORDANCE WITH THE REQUIREMENTS OF THE TOWN OF FRASER STREET AND ROADWAY STANDARD AND GRAND COUNTY STORM DRAINAGE DESIGN AND TECHNICAL CRITERIA MANUAL. THE SIGNATURE AND STAMP AFFIXED HEREON CERTIFIES THAT THESE CONSTRUCTION PLANS WAS PREPARED IN ACCORDANCE WITH THE REQUIRED REGULATIONS AND CRITERIA; HOWEVER, THE STAMP AND SIGNATURE DOES NOT CERTIFY OR GUARANTEE FUTURE PERFORMANCE OF THE EXECUTION OF THE PLAN BY THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR EXECUTING THE CONSTRUCTION WORK ACCORDING TO THE INFORMATION SET FORTH IN THE PLAN AND IN ACCORDANCE WITH ALL APPLICABLE REQUIREMENTS."

LONNY PHELPS

REGISTERED PROFESSIONAL ENGINEER STATE OF COLORADO NO. 31346 PHELPS ENGINEERING

NDEX

SHEET TITLE

8600 8600 8600	EXISTING CONTOURS
8600	PROPOSED CONTOURS
	PROPOSED CONTOURS (BY OTHERS)
	PROPOSED STORM SEWER W/INLET AND FES
	EXISTING STORM SEWER
	EASEMENT
V	PROPOSED RETAINING WALL
	PROPOSED TRAIL
	WETLANDS
	RIPRAP
	PROPOSED WATER LINE W/FIRE HYDRANT
W	EXISTING WATER LINE W/FIRE HYDRANT
	PROPOSED SANITARY SEWER W/MANHOLE
►S	EXISTING SANITARY SEWER W/MANHOLE
	FLOWLINE
	CATCH CURB

TODE HANDARY SHE STOL DANK, CO 80204 309-209-144
PRELIMINARY NOT FOR CONSTRUCTION
SCALE: AS SHOWN
DESIGNED BY: FGF DRAWN BY: JAM CHECKED BY: LEP
COVER SHEET
KINGS CROSSING ROAD
DATE 06/20/14 PROJECT_NO
PROJECT NO. 14095
of 6

BE CONSTRUED FOR ANY REASON AS
OWN OF FRASER OR ANY OF THE
FIONAL QUANTITIES OF ITEMS SHOWN
PHASE.

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DATE

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> **BASIS OF BEARING:** THE WEST LINE OF THE NORTHWEST QUARTER OF THE SOUTHWEST QUARTER OF SECTION 28, TOWNSHIP 1 SOUTH, RANGE 75 WEST OF THE SIXTH PRINCIPAL MERIDIAN WHICH WAS ASSUMED TO BEAR SOO"17'02"E.

BENCHMARK:

THE WEST QUARTER CORNER OF SECTION 28, TOWNSHIP 1 SOUTH, RANGE 75 WEST OF THE SIXTH PRINCIPAL MERIDIAN; FOUND 1-1/2" STEEL PIPE WITH 3-1/4" GLO BRASS CAP MARKED "1933", ELEVATION = 8689.29 U.S. SURVEY FEET, NGVD29 DATUM.



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2.	NERAL NOTES: THE CONTRACTOR SHALL OBTAIN, AT HIS EXPENSE, ALL PERMITS WHICH ARE NECESSARY TO PERFORM THE PROPOSED WORK.	<u>PIP</u> C. BE
	ALL BACKFILL MATERIAL SHALL BE COMPACTED TO 95% STANDARD PROCTOR DENSITY AND ADJUSTED FOR PROPER MOISTURE	С. Бі 1.
	CONTENT.	
3.	TRENCHES SHALL BE EXCAVATED AND THE PIPE EXPOSED FOR INSPECTION AT ANY LOCATION ON THE PROJECT IF SO ORDERED.	
4.	ALL STATIONING IS ALONG THE CENTERLINE OF THE ROADWAY UNLESS OTHERWISE NOTED.	
5.	THE PROFILE GRADE ON THE PLANS IS THE ROADWAY CENTERLINE UNLESS OTHERWISE NOTED.	
6.	THE CONTRACTOR SHALL HAVE ON HIS POSSESSION AT THE SITE A COPY OF THE APPROVED CONSTRUCTION PLANS.	I
	LIMITS OF WORK: NO AREAS SHALL BE DISTURBED OUTSIDE OF THE TEMPORARY CONSTRUCTION EASEMENTS AND THE ROADWAY DISTURBANCE LIMITS.	
	WETLAND MITIGATION SHALL BE COORDINATED WITH WESTERN ECOLOGICAL RESOURCES, INC.	2.
	ALL SANITARY SEWER CONSTRUCTION SHALL COMPLY WITH CHAPTER 14 - DESIGN CRITERIA AND CONSTRUCTION STANDARDS IN FRASER'S CODE. THE TOWN OF FRASER OPERATES AND MAINTAINS THE MUNICIPAL SANITARY SEWER COLLECTION SYSTEM. ALL WORKMANSHIP SHALL BE SUBJECT TO INSPECTION BY THE DEVELOPER, TOWN OF FRASER, OR THEIR REPRESENTATIVES. ONE OR ALL OF THE PARTIES HAS THE RIGHT TO REJECT MATERIALS AND WORKMANSHIP WHICH DO NOT COMPLY.	3.
	THE CONTRACTOR SHALL NOTIFY THE TOWN OF FRASER AND THE PUBLIC UTILITY COMPANIES PRIOR TO PROCEEDING WITH ANY EXCAVATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ANY EXISTING UTILITY (INCLUDING DEPTHS) WHICH MAY CONFLICT WITH THE PROPOSED CONSTRUCTION. ALL EXISTING UTILITIES SHALL BE PROTECTED FROM DAMAGE BY THE CONTRACTOR. DAMAGED UTILITIES SHALL BE REPAIRED BY THE CONTRACTOR AT HIS OWN EXPENSE. ALL ITEMS SHOWN ON THE PLANS AS EXISTING ARE SHOWN IN APPROXIMATE LOCATIONS ONLY. THE ACTUAL LOCATIONS MAY VARY FROM THE PLANS, ESPECIALLY IN THE CASE OF UNDERGROUND UTILITIES. WHENEVER THE CONTRACTOR DISCOVERS A DISCREPANCY IN LOCATIONS, THE CONTRACTOR SHALL CONTACT THE ENGINEER IMMEDIATELY. ALL WORK PERFORMED IN THE AREA OF THE PUBLIC UTILITIES SHALL BE PERFORMED ACCORDING TO THE REQUIREMENTS OF THESE AGENCIES.	
	CONTRACTOR SHALL GIVE 72 HOURS NOTICE TO TOWN PERSONNEL TO PERFORM REQUIRED INSPECTIONS AND PRIOR TO ANY CONSTRUCTION ON THIS SITE. CONTACT TOWN OF FRASER PERSONNEL AT (970) 726-5491.	4.
ST	ORM SEWER NOTES:	
	LOCATION OF EXISTING STORM SEWER (INCLUDING CULVERTS) SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO START OF CONSTRUCTION.	
2.	STORM SEWER SHALL BE RCP PER CHAPTER 14 - DESIGN CRITERIA AND CONSTRUCTION STANDARDS IN FRASER'S CODE.	5.
	ALL CULVERTS SHALL HAVE END SECTIONS ON BOTH THE UPSTREAM AND DOWNSTREAM ENDS OF THE PIPE UNLESS OTHERWISE NOTED ON THE PLANS.	
	STORM SEWER SHALL BE RCP AND SHALL HAVE BEDDING AND BACKFILL PER CHAPTER 14 - DESIGN CRITERIA AND CONSTRUCTION STANDARDS IN FRASER'S CODE.	6.
	PIPE LENGTHS FOR STORM SEWER ARE APPROXIMATE HORIZONTAL DISTANCES FROM END SECTION TO END SECTION. THEREFORE, DISTANCES SHOWN ON THE PLANS ARE APPROXIMATE ONLY AND COULD VARY. END SECTIONS ARE INCLUDED IN THE PIPE LENGTH SHOWN ON THE PLANS.	
	DRIVEWAYS FOR ADJACENT PARCELS AND CULVERTS UNDER THE DRIVEWAYS SHALL BE DESIGNED IN THE FUTURE IN CONJUNCTION WITH DESIGNS FOR THE ADJACENT PARCELS.	
<u>IF</u>	PE BEDDING AND BACKFILL:	<u>RO</u>
	A. TRENCH ZONES. THE TERMS "BEDDING ZONE", "PIPE ZONE" AND "BACKFILL ZONE" SHALL REFER TO THE TRENCH ZONES	1.
	IDENTIFIED IN THE STANDARD DETAILS, WATER MAIN BEDDING AND BACKFILL DETAIL.	2.
	1. BEDDING ZONE. THE BEDDING ZONE SHALL CONSIST OF ALL MATERIAL PLACED BELOW THE PIPE INVERT OR, WHEN PERMITTED, THE NATIVE MATERIALS GRADED AND PREPARED FOR DIRECT PLACEMENT OF THE PIPE.	
	2. PIPE ZONE. THE PIPE ZONE SHALL CONSIST OF ALL MATERIAL PLACED ABOVE THE PIPE INVERT TO AN ELEVATION SHOWN ON	3.
	THE DETAILS. 3. BACKFILL ZONE. THE BACKFILL ZONE SHALL CONSIST OF ALL MATERIAL ABOVE THE PIPE ZONE.	4.
	 BACKFILL ZONE. THE BACKFILL ZONE SHALL CONSIST OF ALL MATERIAL ABOVE THE PIPE ZONE. B. MATERIAL. ALL BEDDING AND BACKFILL MATERIAL SHALL HAVE THE APPROVAL OF THE ENGINEER. ALL BEDDING AND BACKFILL 	5.
	D. MATERIAL. ALL BEDDING AND BACKFILL MATERIAL SHALL HAVE THE APPROVAL OF THE ENGINEER. ALL BEDDING AND BACKFILL MATERIAL SHALL HAVE THE APPROVAL OF THE ENGINEER. ALL BEDDING AND BACKFILL MATERIAL, ORGANIC MATERIAL AND DEBRIS. THE MATERIALS TO BE USED IN EACH TRENCH ZONE ARE INDICATED ON THE STANDARD DETAILS AND THESE MATERIALS ARE DESCRIBED BELOW. ALL MATERIALS MAY BE SUBJECT TO GRADATION TESTS AND COMPACTION TESTS PRIOR TO APPROVAL OF THE USE OF THAT MATERIAL. THE TEST RESULTS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL AND VERIFIED AS TO THEIR ACCURACY. THESE TESTS SHALL BE PERFORMED AT NO COST TO THE TOWN OR DISTRICT OR THEIR AGENTS.	6. 7.
	 SHALL BE PERFORMED AT NO COST TO THE TOWN OR DISTRICT OR THEIR AGENTS. 1. GRANULAR BEDDING MATERIAL. THIS MATERIAL SHALL BE A CLEAN, WELL-GRADED GRAVELLY MATERIAL AND SHALL CONFORM TO THE FOLLOWING LIMITS WHEN TESTED BY MEANS OF LABORATORY SIEVES: 	-
:	SIEVE SIZE PASSING BY WEIGHT TOTAL PERCENT	8.
	SIEVE SIZE PASSING BY WEIGHT TOTAL PERCENT 3/8-INCH 100	8.
	3/8-INCH 100 NO. 4 70 - 100 NO. 8 36 - 93	8.
	3/8-INCH 100 NO. 4 70 - 100 NO. 8 36 - 93 NO. 16 20 - 80 NO. 30 8 - 65	8.
	3/8-INCH 100 NO. 4 70 - 100 NO. 8 36 - 93 NO. 16 20 - 80 NO. 30 8 - 65 NO. 50 2 - 30 NO. 100 1 - 10	8.
	3/8-INCH 100 NO. 4 70 - 100 NO. 8 36 - 93 NO. 16 20 - 80 NO. 30 8 - 65 NO. 50 2 - 30 NO. 100 1 - 10 NO. 200 0 - 3	8.
	3/8-INCH100NO. 470 - 100NO. 836 - 93NO. 1620 - 80NO. 308 - 65NO. 502 - 30NO. 1001 - 10NO. 2000 - 32.ROADBASE BEDDING MATERIAL OR ROADBASE BACKFILL. THIS MATERIAL SHALL BE CLASS 6 AGGREGATE BASE COURSE AS SPECIFIED BY THE STATE OF COLORADO DEPARTMENT OF HIGHWAYS; AND SHALL MEET THE FOLLOWING GRADATION:	8.
	3/8-INCH 100 NO. 4 70 - 100 NO. 8 36 - 93 NO. 16 20 - 80 NO. 30 8 - 65 NO. 50 2 - 30 NO. 100 1 - 10 NO. 200 0 - 3 2. ROADBASE BEDDING MATERIAL OR ROADBASE BACKFILL. THIS MATERIAL SHALL BE CLASS 6 AGGREGATE BASE COURSE AS SPECIFIED BY THE STATE OF COLORADO DEPARTMENT OF HIGHWAYS; AND SHALL MEET THE FOLLOWING GRADATION: SIEVE SIZE PASSING BY WEIGHT TOTAL PERCENT	8.
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PE BEDDING AND BACKFILL (CON'T):

BEDDING AND BACKFILL INSTALLATION:

GENERAL. UNLESS ACCURATE RESULTS CANNOT BE OBTAINED, THE COMPACTION REQUIREMENTS SHALL CONFORM TO MAXIMUM DRY DENSITY ACCORDING TO ASTM D698, MOISTURE-DENSITY RELATIONS OF SOILS (STANDARD PROCTOR). WHEN THE ASTM D698 TEST IS NOT APPLICABLE, THE PERCENTAGE COMPACTION REQUIREMENTS SHALL CONFORM TO ASTM D2049, TEST FOR RELATIVE DENSITY OF COHESIONLESS SOILS.

WHEN REQUIRED BY THE ENGINEER, THE DEVELOPER SHALL EXCAVATE BACKFILLED TRENCHES FOR THE PURPOSE OF PERFORMING COMPACTION TESTS AT LOCATIONS AND DEPTHS REQUIRED BY THE ENGINEER. THE DEVELOPER SHALL BE RESPONSIBLE FOR REINSTALLING AND COMPACTING THE TEST EXCAVATIONS AT NO ADDITIONAL COST TO THE TOWN.

IN AREAS UNDER ROADWAYS AND IN PUBLIC RIGHT-OF-WAY, COMPACTION TESTS SHALL BE PERFORMED IN THE TRENCH FOR THE BEDDING AND IN THE BACKFILL ZONE, ONE AND ONE-HALF (1 ½) FEET ABOVE THE TOP OF PIPE AND IN ONE-FOOT VERTICAL INCREMENTS TO FINISH GRADE. COMPACTION TEST SHALL BE PERFORMED FOR EACH VERTICAL INCREMENT NOTED ABOVE AND AT HORIZONTAL INTERVALS OF EVERY ONE HUNDRED (100) FEET, MEASURED ALONG THE CENTERLINE OF PIPE.

BEDDING ZONE INSTALLATION. BEDDING MATERIAL SHALL CONSIST OF THE MATERIAL ON WHICH THE PIPE IS PLACED IN ACCORDANCE WITH THE PIPE TRENCH DETAILS. BEDDING MATERIAL SHALL BE PLACED TO THE REQUIRED ELEVATION OF THE PIPE INVERT. TAMPING EQUIPMENT SHALL BE USED TO THOROUGHLY TAMP THE BEDDING MATERIAL TO A MINIMUM OF 95 PERCENT MAXIMUM DRY DENSITY OR TO 75 PERCENT RELATIVE DENSITY. THE MOISTURE CONTENT OF THE MATERIAL SHALL BE WITHIN 2 PERCENT OF OPTIMUM.

PIPE ZONE INSTALLATION. AFTER BEDDING MATERIAL HAS BEEN PLACED AND APPROVED AND AFTER THE PIPE HAS BEEN INSTALLED AND APPROVED, THE PIPE ZONE BACKFILL SHALL BE INSTALLED TO AN ELEVATION SHOWN ON THE DETAILS ON THE DRAWINGS.

THE BACKFILL MATERIAL SHALL BE AS SPECIFIED ON THE DETAILS AND SHALL BE PLACED AND COMPACTED IN DISTINCT, SEPARATE LIFTS NOT TO EXCEED 6 INCHES OF LOOSE DEPTH; EXCEPT THAT THE FIRST LOOSE LIFT SHALL NOT BE HIGHER THAN THE PIPE CENTERLINE (SPRINGLINE). IF SELECT BACKFILL MATERIALS ARE PERMITTED IN THIS ZONE BUT ACCEPTABLE SELECT BACKFILL MATERIAL (SUITABLE FOR PLACEMENT WITHIN 12 INCHES OF THE PIPE BARREL) IS NOT AVAILABLE AT ANY PARTICULAR LOCATION, THE DEVELOPER SHALL USE IMPORTED GRANULAR BACKFILL MATERIAL. COMPACTION SHALL MEET THE REQUIREMENTS OF "BEDDING ZONE INSTALLATION," UTILIZING T-BARS OR MECHANICAL TAMPING EQUIPMENT.

BACKFILL ZONE INSTALLATION: OUTSIDE OF THE PUBLIC RIGHT-OF-WAY AND NOT UNDER DRIVEWAYS, STREETS, AND PARKING LOTS. AFTER THE PIPE ZONE BACKFILL HAS BEEN PLACED AND APPROVED, THE TRENCH SHALL BE BACKFILLED. ALL BACKFILL ABOVE THE PIPE ZONE BACKFILL SHALL BE CAREFULLY PLACED IN THE TRENCH IN LIFTS NO GREATER THAN EIGHTEEN (18) INCHES. EACH LIFT SHALL BE COMPACTED BY MECHANICAL EQUIPMENT TO NINETY PERCENT (90%) OF STANDARD DRY DENSITY. AFTER THE TRENCH IS BACKFILLED TO THE GROUND SURFACE, A LOADED DUMP TRUCK OR LOADER PLACED IN THE TRENCH LINE SHALL COMPACT THE BACKFILL BY ITS WHEEL LOAD. NO FEWER THAN TWO (2) PASSES SHALL BE MADE. IF THE BACKFILL IS DEPRESSED BELOW THE FINISHED GRADE ELEVATION, THE DEPRESSED AREA SHALL BE REFILLED AND COMPACTED. THE BACKFILL SHALL BE MOUNDED HIGHER THAN ADJACENT GROUND TO ALLOW FOR SETTLEMENT.

BACKFILL ZONE INSTALLATION IN ROADS AND STREETS. BENEATH ALL TRAVELED WAYS IN ROADS AND STREETS, HIGHWAY SHOULDERS AND WITHIN 15 FEET OF PAVEMENT IN STATE HIGHWAY DEPARTMENT RIGHTS-OF-WAY (UNLESS OTHERWISE SPECIFIED ON THE PLANS) BACKFILL SHALL BE CAREFULLY PLACED AND COMPACTED UP TO THE LIMIT OF BASE COURSE MATERIAL OR TO GRAVEL. COMPACTION SHALL BE BY MECHANICAL TAMPING IN 8-INCH MAXIMUM LOOSE LIFTS USING MECHANICAL OR HAND TAMPERS, WEIGHING NOT LESS THAN 20 POUNDS, OR VIBRATORY ROLLERS. ALL OTHER MEANS MUST BE APPROVED IN WRITING BY THE ENGINEER. ALL BACKFILL SHALL BE COMPACTED TO 95 PERCENT OF MAXIMUM LABORATORY DRY DENSITY OR 70 PERCENT RELATIVE DENSITY. THE MATERIAL SHALL BE WITHIN 2.0 PERCENT OF OPTIMUM MOISTURE CONTENT.

THE DEVELOPER MAY REQUEST APPROVAL OF ALTERNATE MEANS OF COMPACTION. SUCH REQUEST MUST BE SUBMITTED TO THE ENGINEER IN WRITING. APPROVAL OF THE COMPACTION METHOD WILL BE MADE BY THE ENGINEER ONLY IN WRITING. USE OF SPECIFIED OR APPROVED COMPACTION METHODS DOES NOT RELIEVE THE DEVELOPER FROM PROVIDING A COMPLETED PROJECT MEETING THE INTENT OF THIS SPECIFICATION.

DADWAY NOTES:

LIFTS IN FILL AREAS SHALL NOT EXCEED 8 INCHES IN COMPACTED DEPTH. MAXIMUM SLOPES OF ALL CUTS & FILLS SHALL BE 3:1 UNLESS OTHERWISE NOTED.

PAVING SHALL NOT START UNTIL SUBGRADE COMPACTION TESTS ARE TAKEN MEETING THE REQUIREMENTS OF THE PLANS AND SPECS AND FINAL PAVEMENT DESIGN. THE PAVEMENT SECTION SHALL BE IN ACCORDANCE WITH THE GEOTECHNICAL REPORT FOR THIS PROJECT. THE MINIMUM DEPTH OF ASPHALT SHALL BE 5-INCHES.

THE CONTRACTOR SHALL SAW-CUT ALL EXISTING PAVEMENT WHERE MATCH LINES WITH EXISTING EDGE OF PAVEMENT OCCUR.

PORTLAND CEMENT CONCRETE SHALL BE CONSTRUCTED IN ACCORDANCE WITH CDOT STANDARD 600.

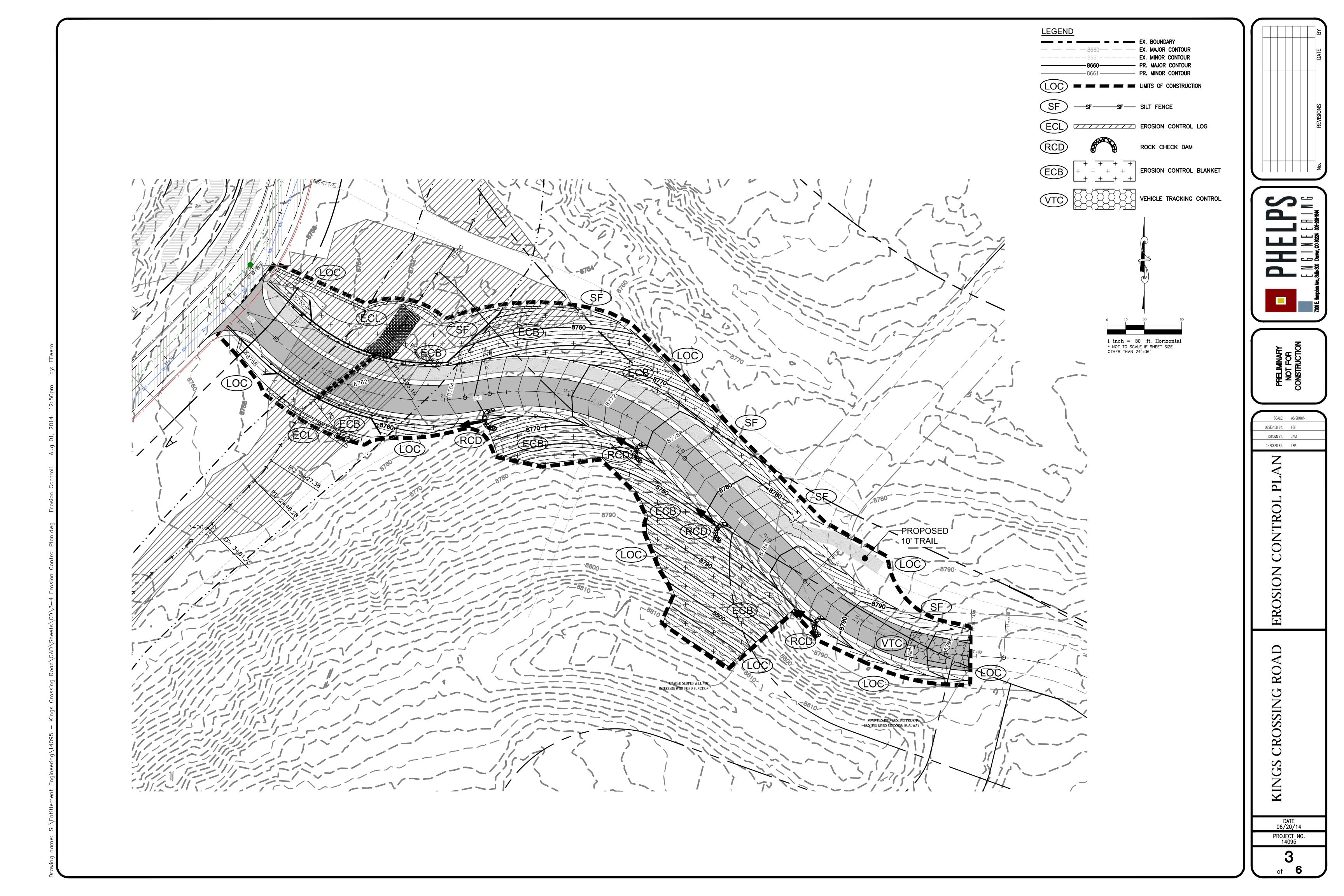
ASPHALT PAVING SHALL BE IN ACCORDANCE WITH THE TOWN OF FRASER ROAD STANDARDS FINAL ASPHALT THICKNESS AND BASE COURSE THICKNESS SHALL BE IN ACCORDANCE WITH THE SOIL'S ENGINEER'S REPORT.

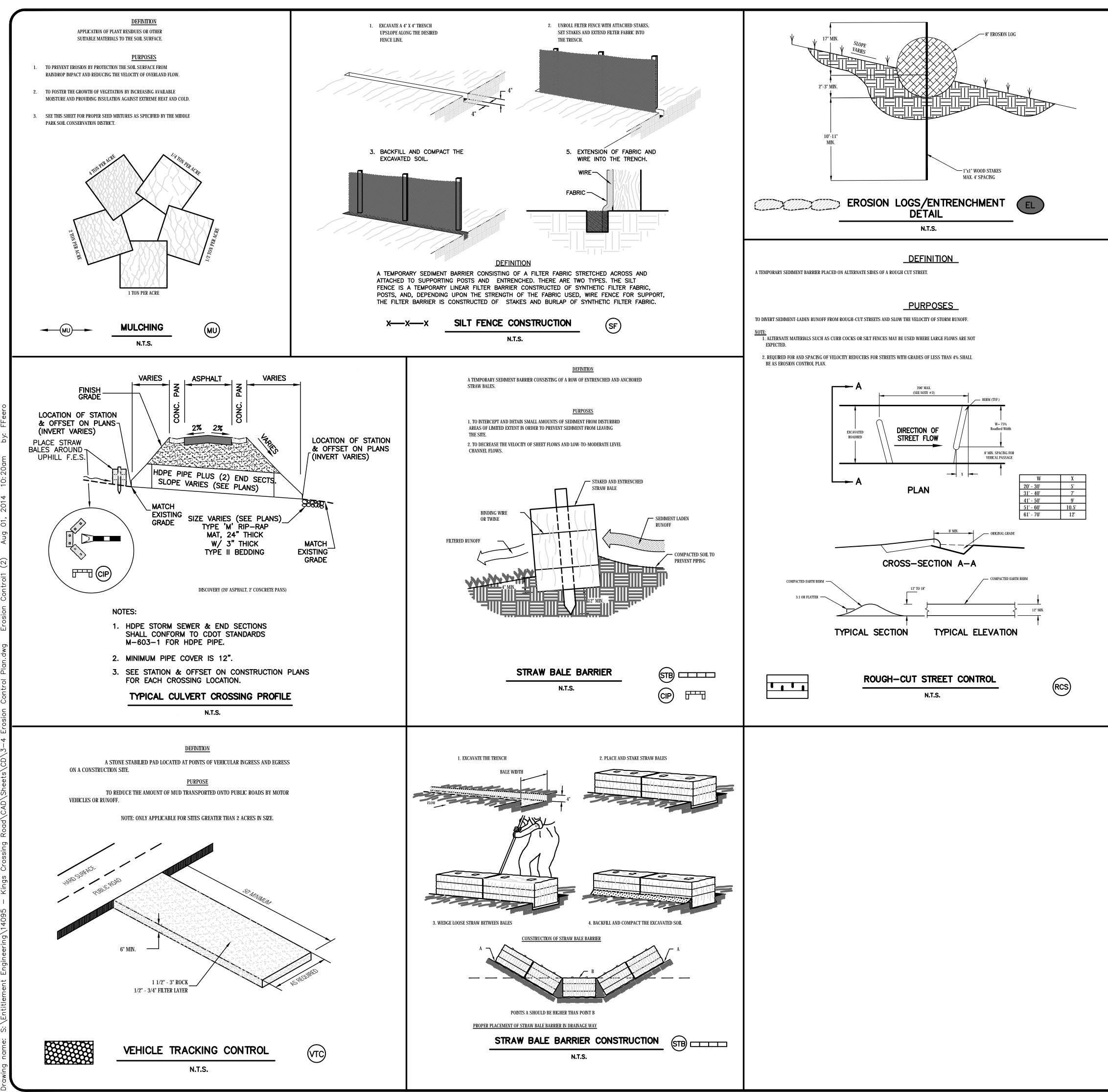
ROADWAY BASE COURSE AND FILL AREA COMPACTION SHALL CONFORM TO THE TOWN OF FRASER ROAD STANDARDS. THE BASE COURSE AND COMPACTION SHALL ALSO CONFORM TO THE GENERAL NOTES.

COMPACTION FOR THE ROADWAY BASE COURSE SHALL MEET 95% OF STANDARD PROCTOR DENSITY WITH THE MATERIAL BEING WITHIN 2.0% OF OPTIMUM MOISTURE CONTENT. EACH LIFT OF ASPHALT SHALL MEET THE MINIMUM DENSITY OF 95% OF THE MARSHALL DENSITY. ASPHALT DENSITY TESTING SHALL BE PERFORMED ON EACH LIFT AT INTERVALS OF ONE TEST PER EVERY 250 LINEAR FEET PER LANE. TEST LOCATIONS ON EACH LIFT AND EACH LANE SHALL BE STAGGERED.

AT COMPLETION OF CONSTRUCTION, AS PART OF THE PRELIMINARY ACCEPTANCE, THE TOWN WILL SELECT REPRESENTATIVE LOCATIONS TO TAKE ASPHALT CORINGS AS CONFIRMATION OF ASPHALT DEPTH AND CONSISTENCY OF THE ASPHALT SECTION. THE TOWN WILL CONTRACT DIRECTLY WITH A COMPANY TO PERFORM THIS WORK AND WILL BACK CHARGE THE DEVELOPER FOR THE COST.

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EROSION CONTROL NOTES:

1. SILT FENCE SHALL BE INSTALLED BY THE CONTRACTOR AND INSPECTED TWENTY-FOUR HOURS PRIOR TO THE START OF GRADING. INLET PROTECTION SHALL BE APPLIED TO ALL EXISTING INLETS PRIOR TO GRADING START-UP.

2. AT ALL TIMES DURING CONSTRUCTION OF THIS PROJECT, EROSION CONTROL AND SEDIMENTATION CONTROL SHALL BE MAINTAINED TO PREVENT ERODED SOIL FROM LEAVING THE PROPERTY AND/OR DISTURBING WETLANDS.

3. ANY SETTLEMENT OR SOIL ACCUMULATIONS BEYOND THE PROPERTY LIMITS DUE TO GRADING OR EROSION SHALL BE REPAIRED IMMEDIATELY BY THE CONTRACTOR. 4. A WATER TRUCK SHALL BE UTILIZED DURING EARTHWORK OPERATIONS AS REQUIRED TO MINIMIZE DUST FROM EARTHWORK EQUIPMENT AND WIND.

5. A LAYER OF SUITABLE MULCH SHALL BE APPLIED TO ALL RESEEDED PORTIONS OF THE SITE WITHIN FOURTEEN DAYS OF THE COMPLETION OF RESEEDING. SAID MULCH SHALL BE APPLIED AT A RATE OF TWO TONS PER ACRE AND SHALL BE TACKED OR FASTENED BY AN APPROVED METHOD SUITABLE FOR THE TYPE OF MULCH USED.

6. SOILS THAT WILL BE STOCKPILED FOR MORE THAN THIRTY DAYS SHALL BE MULCHED AND SEEDED WITH A TEMPORARY OR PERMANENT GRASS COVER WITHIN FOURTEEN DAYS OF STOCKPILE CONSTRUCTION. IF STOCKPILES ARE LOCATED WITHIN 100 FEET OF A DRAINAGEWAY, ADDITIONAL SEDIMENT CONTROLS SUCH AS TEMPORARY DIKES OR SILT FENCES SHALL BE REQUIRED.

7. ALL INLETS WILL HAVE GRAVEL FILTERS INSTALLED PRIOR TO GRADING START-UP TO REDUCE SEDIMENT INTO THE STORM SEWER CROSSINGS. INLET PROTECTION SHALL BE APPLIED IMMEDIATELY TO NEWLY INSTALLED FLARED-END SECTIONS.

8. APPROVED EROSION AND SEDIMENT CONTROL "BEST MANAGEMENT PRACTICES" SHALL BE MAINTAINED AND KEPT IN GOOD REPAIR FOR THE DURATION OF THIS PROJECT. AT A MINIMUM, THE CONTRACTOR OR HIS AGENT SHALL INSPECT ALL BMPS WEEKLY AND AFTER SIGNIFICANT PRECIPITATION EVENTS. ALL NECESSARY MAINTENANCE AND REPAIR SHALL BE COMPLETED IN A TIMELY MANNER. ACCUMULATED SEDIMENT AND DEBRIS SHALL BE REMOVED FROM A BMP WHEN THE SEDIMENT LEVEL REACHES ONE-HALF THE HEIGHT OF THE BMP OR AT ANY TIME THAT SEDIMENT OR DEBRIS ADVERSELY IMPACTS THE FUNCTIONING OF THE BMP.

9. THE CONTRACTOR MUST KEEP ALL POLLUTANTS, INCLUDING TRENCH BACKFILL MATERIALS, OIL, GREASE, ETC. FROM WASHING INTO THE STORM SEWER SYSTEM OR WATERWAYS.

10. THE OWNER, SITE DEVELOPER, CONTRACTOR, AND/OR THEIR AUTHORIZED AGENTS SHALL REMOVE ALL SEDIMENT, MUD, AND CONSTRUCTION DEBRIS THAT MAY ACCUMULATE IN THE DRAINAGEWAYS AND PUBLIC RIGHTS-OF-WAY AS A RESULT OF THIS SITE DEVELOPMENT. SAID REMOVAL SHALL BE CONDUCTED IN A TIMELY MANNER. THE PROJECT OWNER CORNERSTONE WINTER PARK HOLDINGS, LLC. SHALL BE CONTACTED (970-726-8600) IF NECESSARY TO ENFORCE THIS REQUIREMENT.

11. THE CONTRACTOR SHALL BE HELD RESPONSIBLE FOR REMEDIATION OF ANY ADVERSE IMPACTS TO ADJACENT WATERWAYS, WETLANDS, ETC. RESULTING FROM WORK DONE AS PART OF THIS PROJECT.

12. THE GENERAL CONTRACTOR, GRADING CONTRACTOR, AND/OR THEIR AUTHORIZED AGENTS SHALL ENSURE THAT ALL LOADS OF CUT AND FILL MATERIAL IMPORTED TO OR EXPORTED FROM THIS SITE SHALL BE PROPERLY COVERED TO PREVENT LOSS OF THE MATERIAL DURING TRANSPORT ON PUBLIC RIGHTS-OF-WAY. THE PROJECT OWNER WINTER PARK HOLDINGS, LLC. SHALL BE CONTACTED (970-726-8600) IF NECESSARY TO ENFORCE THIS REQUIREMENT.

13. THE USE OF REBAR, STEEL STAKES OR STEEL FENCE POSTS FOR STAKING DOWN STRAW OR HAY BALES OR TO SUPPORT SILT FENCING USED AS AN EROSION CONTROL MEASURE IS PROHIBITED.

14. THE EROSION CONTROL MEASURES MAY BE MODIFIED AS SITE CONDITIONS GOVERN. THIS MAY INCLUDE ELIMINATING CERTAIN EROSION CONTROL MEASURES IF THOSE MEASURES BECOME UNNECESSARY.

15. THE CLEANING OF CONCRETE DELIVERY TRUCK CHUTES INTO WATERWAYS IS PROHIBITED ON THE JOB SITE. THE DISCHARGE OF WATER CONTAINING WASTE CONCRETE TO THE STORM SEWER SYSTEM IS PROHIBITED.

16. THE EROSION CONTROL SYSTEM SHALL REMAIN OPERATIONAL UNTIL PAVING OF ROADWAYS IS COMPLETED AND VEGETATION IS RE-ESTABLISHED. 17. ANY DISTURBED AREAS WHICH ARE NOT TO BE LANDSCAPED OR CONSTRUCTED UPON SHALL BE MULCHED AND RESEEDED USING SEED MIXTURES AS

SPECIFIED IN THE TABLE SHOWN HEREON PREPARED BY THE MIDDLE PARK SOIL CONSERVATION DISTRICT.

REVEGETATION & WEED CONTROL NOTES:

1. SOIL STABILIZATION SHALL BE IMPLEMENTED ANY TIME A DISTURBED AREA IS TO REMAIN EXPOSED AND INACTIVE FOR MORE THAN 14 DAYS (TWO WEEKS). MULCHING SHALL OCCUR IMMEDIATELY AFTER REVEGETATION.

2. A LAYER OF SUITABLE MULCH SHALL BE APPLIED TO ALL RESEEDED PORTIONS OF THE SITE WITHIN FOURTEEN DAYS OF THE COMPLETION OF RESEEDING. SAID MULCH SHALL BE APPLIED AT A RATE OF TWO TONS PER ACRE AND SHALL BE TACKED OR FASTENED BY AN APPROVED METHOD SUITABLE FOR THE TYPE OF MULCH USED. MULCH SHALL BE STRAW OR WEED FREE NATIVE HAY.

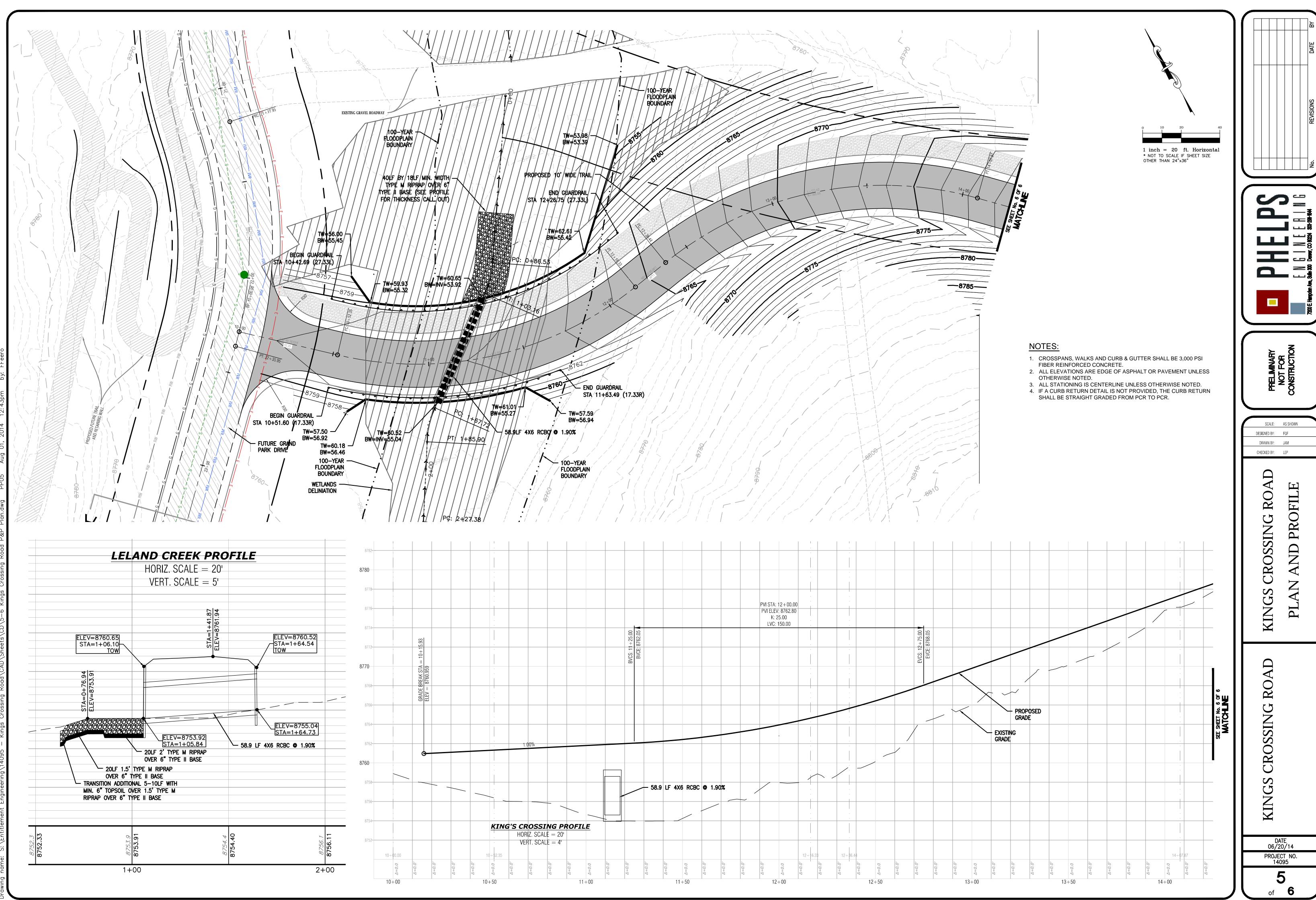
3. SOILS THAT WILL BE STOCKPILED FOR MORE THAN THIRTY DAYS SHALL BE MULCHED AND SEEDED WITH A TEMPORARY OR PERMANENT GRASS COVER WITHIN FOURTEEN DAYS OF STOCKPILE CONSTRUCTION. IF STOCKPILES ARE LOCATED WITHIN 100 FEET OF A DRAINAGEWAY, ADDITIONAL SEDIMENT CONTROLS SUCH AS TEMPORARY DIKES OR SILT FENCES SHALL BE REQUIRED.

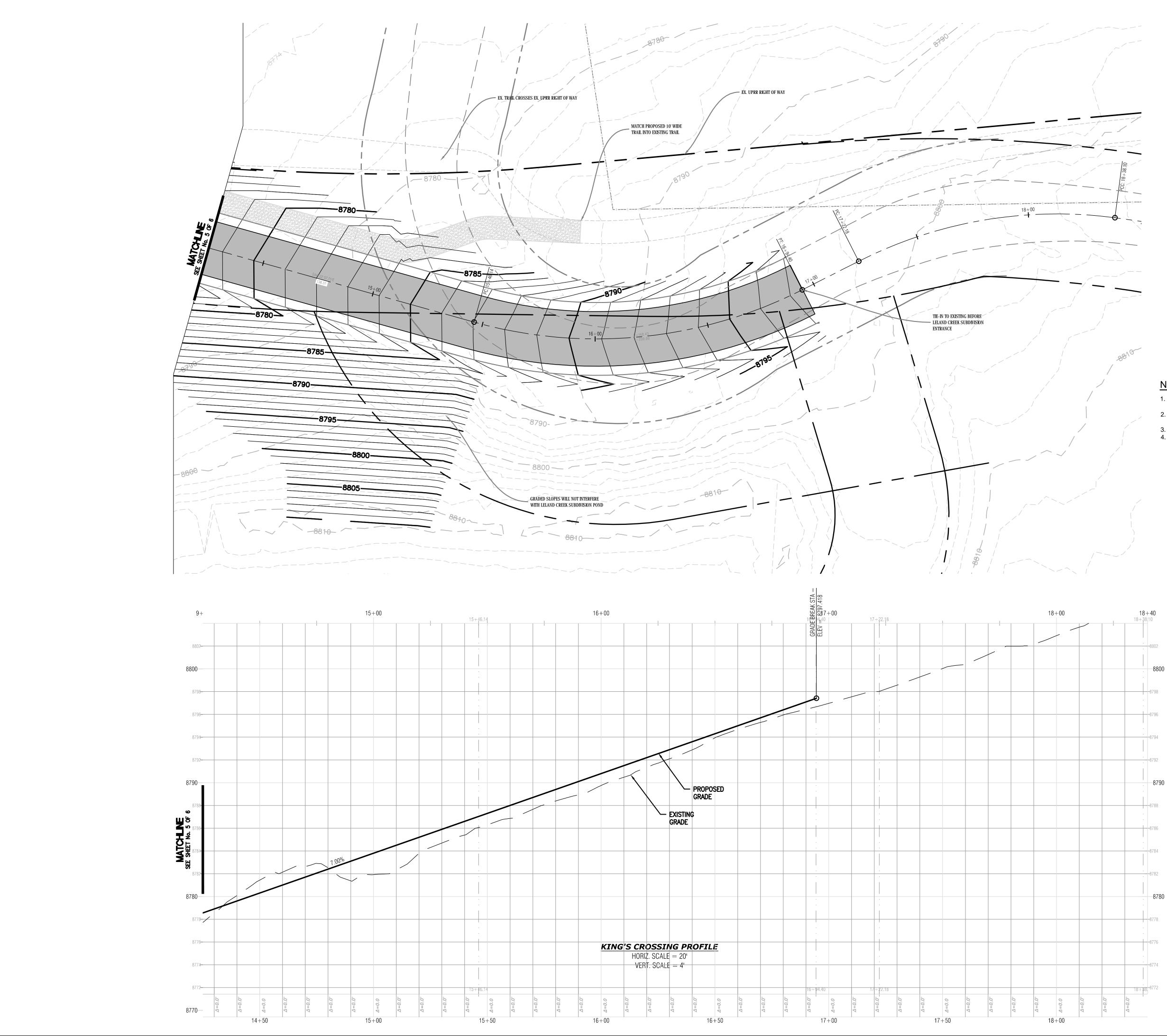
4. ANY DISTURBED AREAS WHICH ARE NOT TO BE LANDSCAPED OR CONSTRUCTED UPON SHALL BE MULCHED AND RESEEDED USING SEED MIXTURES AS SPECIFIED ON THIS SHEET.

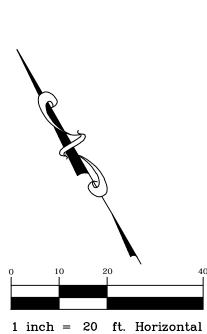
5. NOXIOUS WEED CONTROL SHALL BE APPLIED AS DESCRIBED IN THE NOXIOUS WEED CONTROL PLAN RECORDED AT THE GRAND COUNTY CLERK & RECORDER'S OFFICE AT RECEPTION NUMBER 96003640. WEEDS IDENTIFIED IN THE PLAN INCLUDE: CANADA THISTLE, MUSK THISTLE, LEAFY SPURGE, YELLOW TOADFLAX, HOUNDSTONGUE, MAYWEED, CHAMOMILE AND WHITETOP.

6. REVEGETATION, RE-SEEDING AND WEED CONTROL FOR DISTURBED SURFACE STABILIZATION SHALL CONFORM TO ALL REQUIREMENTS ON THIS SHEET. PERMANENT LANDSCAPE PLANS BY OTHERS MAY SPECIFY ADDITIONAL OR DIFFERENT PLANTINGS AND SURFACE TREATMENTS. PLEASE CONTACT THE NATURAL RESOURCE CONSERVATION SERVICE, (970) 724-3456, FOR QUESTIONS OR MODIFICATIONS TO SEEDING MIXTURES.

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1 inch = 20 ft. Horizontal * NOT TO SCALE IF SHEET SIZE OTHER THAN 24"x36"

NOTES:

- CROSSPANS, WALKS AND CURB & GUTTER SHALL BE 3,000 PSI FIBER REINFORCED CONCRETE.
 ALL ELEVATIONS ARE EDGE OF ASPHALT OR PAVEMENT UNLESS
- ALL ELEVATIONS ARE EDGE OF ASPHALT OR PAVEMENT UN OTHERWISE NOTED.
 ALL STATIONING IS CENTERLINE UNLESS OTHERWISE NOTE
- ALL STATIONING IS CENTERLINE UNLESS OTHERWISE NOTED.
 IF A CURB RETURN DETAIL IS NOT PROVIDED, THE CURB RETURN SHALL BE STRAIGHT GRADED FROM PCR TO PCR.

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